Aviation Language Proficiency **Testing**

Aspeq provides ICAO compliant Aviation Language Proficiency (ALP) testing that is trusted to ensure Pilots and Air Traffic Controllers have the competency to communicate effectively for aeronautical telephony communications.

We have been providing ALP tests for the aviation industry since 2008.

We currently service:

› Civil Aviation Safety Authority of Australia (CASA)
› Civil Aviation Authority of New Zealand (CAA NZ)
› Specific airlines as approved by Hong Kong CAD

aspeq

beyond
question

Our Aviation Language Proficiency tests confirm a candidate meets the criteria of a licence endorsement.

The Level 4 (Operational) descriptor is the safest minimum proficiency skill level determined necessary for aeronautical radiotelephony communications and represents the minimum language proficiency level required to be endorsed on a licence.

Tests may be taken at several points in the career of a pilot or air traffic controller including the career development phase. This may include such instances as:

> a screen for pre-training selection,

> a diagnostic tool in a training programme,

> a progress check during training,

> a licensing requirement in fulfilment of Annex 1 requirements, or

> a periodic re-evaluation of proficiency.

The effective use of plain language is vital in routine operational situations in which phraseology provides no "ready-made" form of communication and is especially critical in unusual or emergency situations.

Enhancing aviation safety through high quality assessments that authorities can rely on

Telephone Delivery

Aspeq provides ALP testing via the telephone where candidate's responses to the questions are recorded and rated by trained and experienced Raters against the ICAO standards.

Using the telephone to undertake the ALP test provides a similar exchange as to what the pilots and air traffic controllers experience in a "live" environment.

> Effectively removes the ability for a candidate to influence an assessment through in-person interaction.

> Records all dialogue for rating and remains available as a historic record.

> Provides fairness to all candidates in the manner of delivery through the absence of face-to-face human interaction and thereby eliminating any possibility for a bias that may arise.

> Ensures tests are delivered in identical conditions.

Formal Language Evaluation

We provide the Formal Language Evaluation as a comprehensive ICAO compliant test to ensure the safest minimum proficiency skill level has been achieved.

The Full Language Evaluation (FLE) test consists of both a semi-direct section with recorded questions and a "live" interview section where an Interviewer asks the candidates questions via the telephone.

The FLE test usually takes approximately 20 minutes depending on the interview section of the test. The candidate's answers are recorded and are then rated by multiple Raters.

Suitable for all speakers

FLEs are suitable for all speakers, including non-native speakers and provides a comprehensive evaluation of spoken English language proficiency.

The outcome for an FLE is a either Level 4, 5, 6, or fail.

Comprehensive moderation process

If the two Raters agree on the rating for each category, then the result is confirmed and provided to the candidate.

If the two Raters do not agree on the rating for each category, then the test is passed to a Supervising Rater who will review the test, marks provided by the previous Raters, and confirms the result.

Fully compliant with ICAO standards

Ratings are to be carried out in accordance with ICAO documents and resources:

› Annex 1; to the Convention on International Civil Aviation.

› Document 9835 standards, recommended practices and principles.

Rated against the ICAO Language Proficiency Rating Scale

› Pronunciation

› Vocabulary

› Comprehension

› Fluency

› Interactions

Our process combines a rare mix of people, experience, technology and best practice to deliver results you can rely on, every time

Aviation Language Proficiency Test Development

We have developed ALP tests in both a Pilot and an Air Traffic Controller context.

Developed by experienced Subject Matter Experts

Our Exam Development Team (EDT) is responsible for the design, development, implementation, and ongoing performance of our ALP tests.

Our network of highly experienced Subject Matter Experts have expertise in the use of aviation operational language and linguistic expertise as well as wider aviation assessment.

Performance Monitoring to validate ALP test credibility

Aspeq is committed to ensuring that the quality and relevance of the ALP tests is enhanced over time.

If the test becomes too well known, recognisable or is not sufficiently robust then it may become subject of specific target learning.

Aspeq has a range of processes to ensure that we maintain credibility:

› Evaluating candidate and industry training feedback.

› Monitoring Rater performances, noting for variances.

› Regular and ongoing training of Raters and Interviewers.

› Maintaining appropriate number of questions in the ALP question bank to prevent questions becoming repetitious and overexposed.

Designed to assess plain spoken language in an aviation context

Tests are delivered with the intention of measuring candidates' ability to listen to, and speak, spoken language and are to be non-inclusive of highly technical or industry specific language.

The ALP tests are by voice-only interaction and are absent of discrete-point item and discrete vocabulary testing.

The tests are delivered in spoken and listening models using both pre-recorded (semi-direct) human voice prompts and by (direct) spoken interview with a linguistic expert.
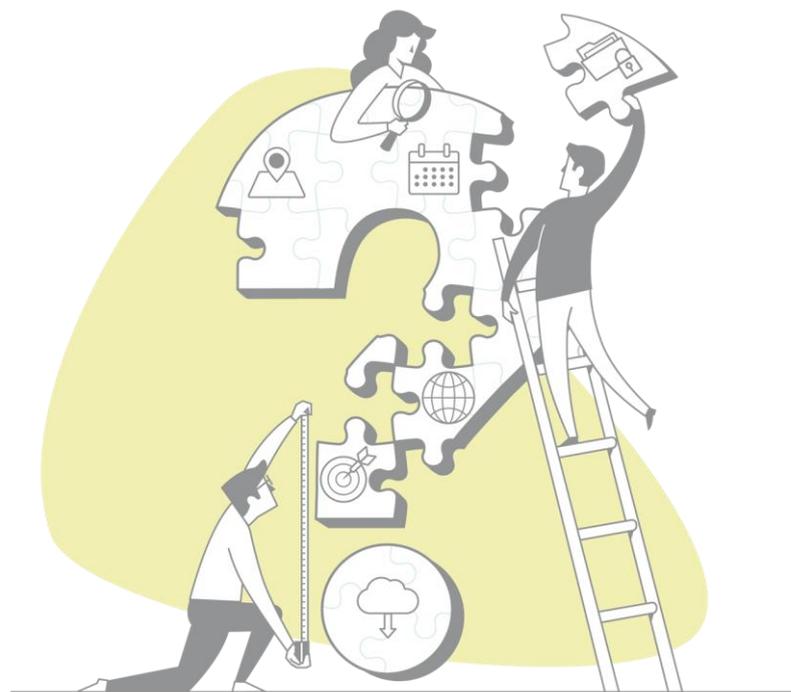
Compliant to ICAO Standards

Our ALP tests are designed to test the candidate's ability to meet ICAO language proficiency requirements.

They have been constructed to comply with ICAO Document 9835; Manual on the Implementation of ICAO Language Proficiency Requirements, and we will ensure they meet your authorities' regulatory requirements.

Constructed to established ICAO criteria

Our tests are constructed in accordance with established ICAO criteria contained within the following ICAO documentation and instructions:

SARPS (Standards and Recommended Practices)

Developed by ICAO and cover all technical and operational aspects of international civil aviation, such as:

›   safety

›   personnel licensing

›   operation of aircraft

›   aerodromes

›   air traffic services

›   accident investigation

›   the environment

Holistic Descriptors

ICAO provides holistic descriptors requiring that proficient speakers shall:

›   communicate effectively in voice-only (telephone/radiotelephone) and in face-to-face situations.

›   communicate on common, concrete and work-related topics with accuracy and clarity.

›   use appropriate communicative strategies to exchange messages and to recognize and resolve misunderstandings (e.g., to check, confirm, or clarify information) in a general or work-related context)

›   handle successfully and with relative ease the linguistic challenges presented by a complication or unexpected turn of events that occurs within the context of a routine work situation or communicative task with which they are not otherwise familiar.

›   use a dialect or accent which is intelligible to the aeronautical community.

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